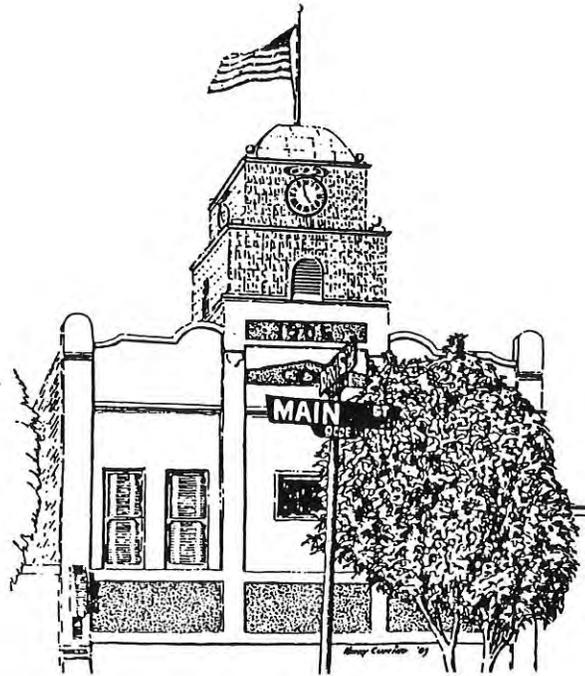


The City of Santa Paula Downtown Improvement Plan



Prepared for:

The City of Santa Paula
970 Ventura Street
Santa Paula, CA 93060

By:

The Downtown Improvement Group

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COMMUNITY OVERVIEW

Introduction

On July 2, 1990, the Santa Paula City Council approved the Redevelopment Plan for the Santa Paula Redevelopment Project. The Project Area includes approximately 1,104 acres in one contiguous land mass. For the most part, the Redevelopment Area had become blighted over time due to the existence of conflicting land uses and economic disuse. The downtown exhibits many of these characteristics due to the age and density of its physical development. The Redevelopment Plan provides a framework for substantially improving these negative conditions.

On June 7, 1993, the Santa Paula City Council approved specific strategies for the development of a Downtown Improvement Plan by way of community involvement. This plan has been developed in recognition of the need to preserve many of the historical buildings and to re-establish the downtown as a viable shopping hub for both tourism and local residents.

The Downtown Improvement Plan has been prepared specifically for the community, by the community. The development of the Downtown Improvement Plan and implementation effort was started because of the inability of private enterprise acting alone to alleviate the poor visual appearance, increasing vacancy rates, non-accessible off-street parking and the continued economic decline of the downtown. This program links physical design proposals to community goals, as well as the socio-economic policies outlined in the City's economic, general and redevelopment plans. Through the coordination of various projects within the area, they are implemented as integrated elements of a complete whole, following urban design and planning principles.

The Santa Paula Downtown Improvement Plan focuses on various key components and incorporates an implementation strategy with design guide. This program establishes a "road map" to achieve revitalization of the downtown.

In the development of the Downtown Improvement Plan, citizen participation was of paramount concern. This concern resulted in the approval of a program, by the City Council, that encouraged citizen/shopper and merchant/owner participation in the planning process. As a result of many hours of dedicated work by the Downtown Improvement Group and Technical Design Team, the specific actions and strategies for downtown revitalization are presented in this report.

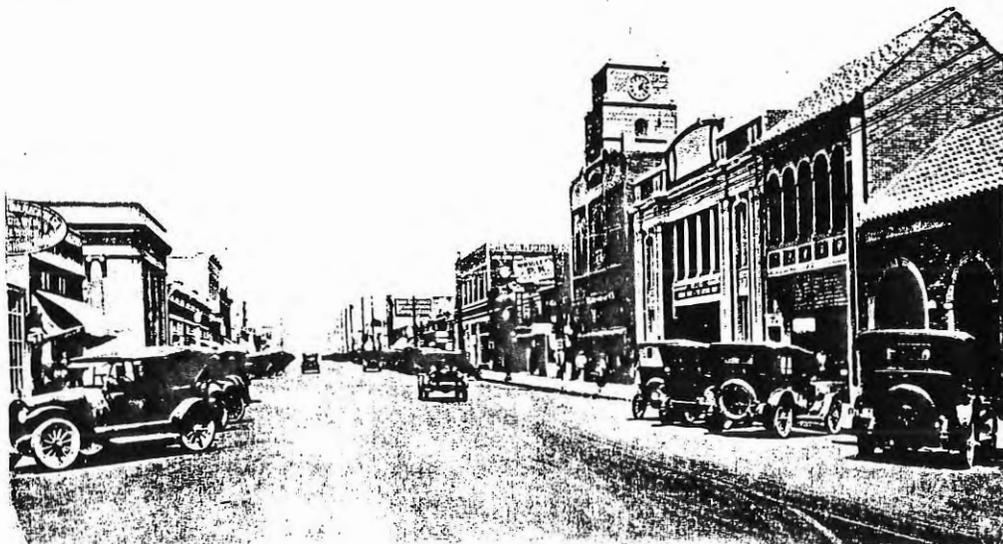
After many meetings of the Downtown Improvement Group, a common ground of understanding was established leading to the development of goals and objectives to guide meaningful change in downtown. During this process, many issues and

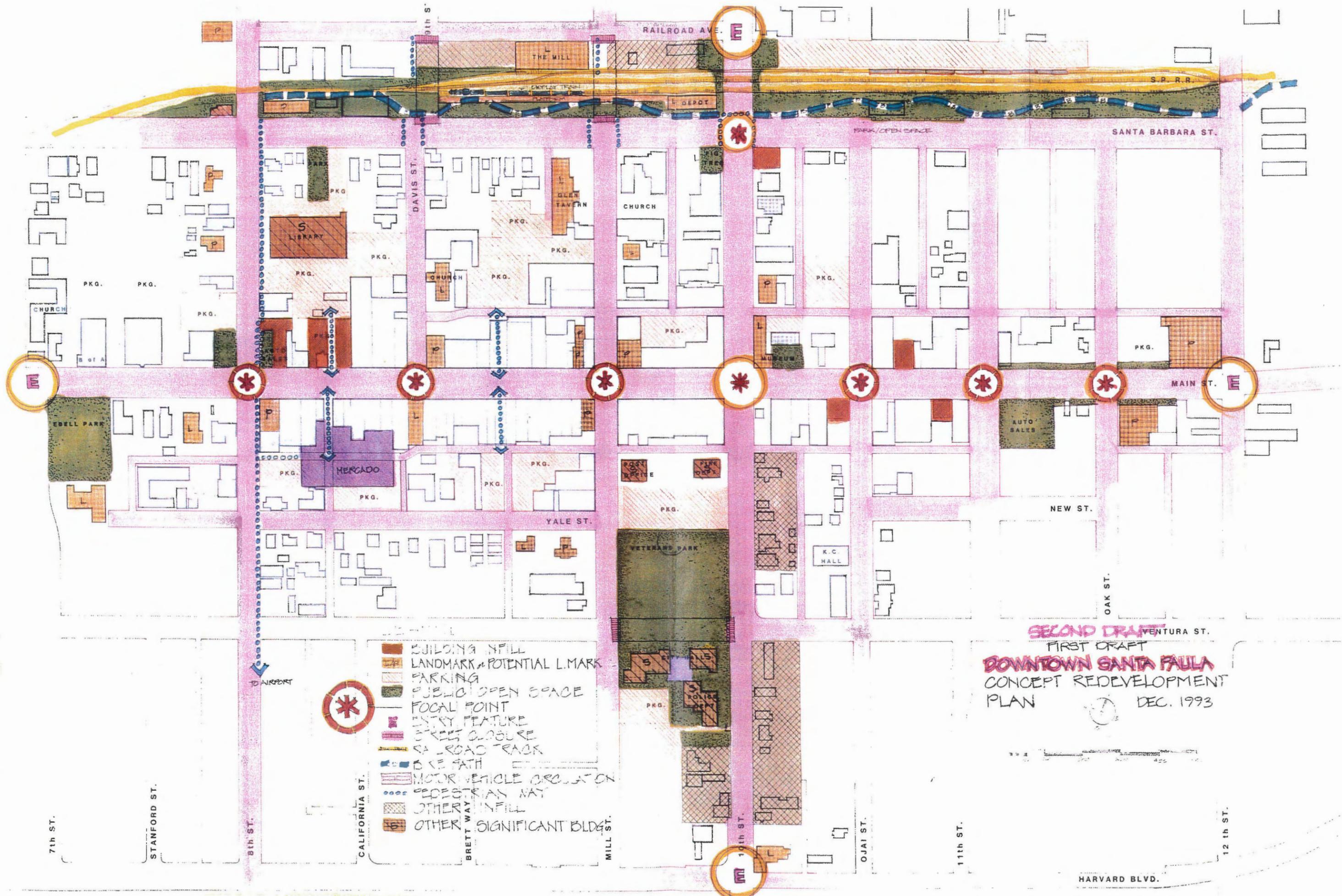
deficiencies were confronted and opportunities identified to achieve revitalization of the downtown. A sincere attitude of enthusiasm and strong desire to improve the declining retail district was clearly evident throughout the planning process, by all participants, and will continue to be necessary during its phased implementation.

For the purposes of this program, the boundaries of downtown Santa Paula are defined as being roughly contained within Railroad Avenue, 7th Street, 12th Street, and Harvard Boulevard. It should be noted that a large portion of the identified boundaries include residential and public land uses. The properties fronting on Main Street and several major cross streets provide for most of the retail establishments.

Although the Downtown Improvement Plan describes generalized boundaries for downtown, the main thrust of the plan will focus on key components or features located within the generalized downtown area. These identified features represent opportunities that can be taken advantage of through the implementation of the downtown improvement plan.

Santa Paula's downtown is experiencing competition from newer and more accessible shopping centers in Ventura County, with the introduction and expansion of modern mall-type shopping centers. For its future development and success, the City should focus its efforts on establishing the downtown as a unique shopping area, emphasizing its historic pedestrian scale, store fronts, streetscapes, and personalized services, as well as museums and other cultural attractions.





- BUILDING FILL
- LANDMARK or POTENTIAL L.MARK
- PARKING
- PUBLIC OPEN SPACE
- FOCAL POINT
- ENTRY FEATURE
- STREET CLOSURE
- RAILROAD TRACK
- BIKE PATH
- MOTOR VEHICLE CIRCULATION
- PEDESTRIAN WAY
- OTHER WAY FILL
- OTHER SIGNIFICANT BLDG.

SECOND DRAFT
 FIRST DRAFT
DOWNTOWN SANTA PAULA
 CONCEPT REDEVELOPMENT
 PLAN
 DEC. 1993



HARVARD BLVD.

History of Downtown Santa Paula

Santa Paula and its environs are part of the Saticoy y Santa Paula Rancho, which was subdivided in 1867 by George Briggs, a horticulturist from Marysville. Mr. Briggs contracted with E.B. Higgins to divide the land into 160 acre plots, which were then used for ranching and farming.

In 1872, Nathan W. Blanchard came to the area and purchased 2700 acres of the Briggs subdivision, including the townsite and Santa Paula Creek water rights. Blanchard, together with his silent partner Elisha Bradley, established the Santa Paula Flour Mill in 1873 and expanded the original townsite in 1875, which helped to encourage growth.

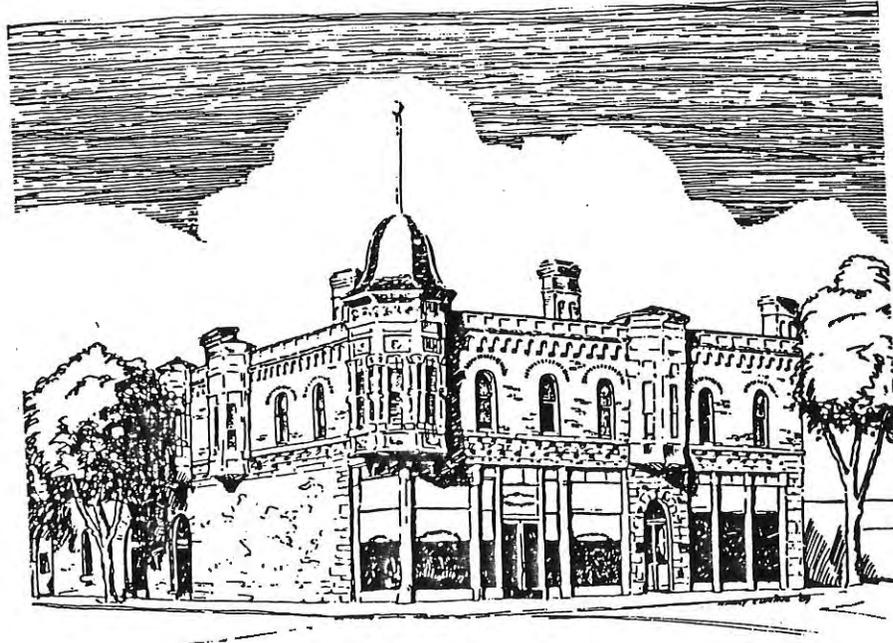
The arrival of the railroad in 1887, coupled with citrus experimentation and oil exploration, resulted in a population boom in Santa Paula typical of those occurring in Southern California in the late 1800's.

During this time the newly opened subdivisions, such as the McKeveatt Tract and the Hardison-Irwin Tract, resulted in the construction of several residences on Santa Paula Street and the surrounding area. Many farm houses were built on the outskirts of town. The growth of the oil industry in Santa Paula with the formation of the Union Oil Company in 1890 and the rise of the large citrus industries, such as the Limoneira Company in 1893, created a wave of prosperity for Santa Paula, which became known as the "Lemon Capital of the World."

The results of this prosperity were seen in the increased development in the residential and commercial areas of the City. Many fine residences were constructed north of Main Street on Santa Paula and Pleasant Streets. More modest houses were built south of Main Street. Houses were built for those working in oil or citrus related industries or for Main Street business owners and their families. Styles include a mixture of Queen Anne, Colonial Revival and California bungalow. The neighborhoods north of Santa Barbara Street, between Palm Avenue on the west and Tenth Street on the east, represent a significant number of intact neighborhoods.

The Main Street commercial area between Tenth and Seventh Streets has undergone several changes over the years. The earliest structures were wood frame buildings from the 1870's, which were replaced by more fashionable brick and stone buildings by the 1890's. In the 1920's and 1930's, several new Spanish Colonial Revival and beaux-arts classical style buildings were constructed, as well as some simple Modern Styles. Many of the structures date from the 1890's, but have modernized store fronts.

Santa Paula incorporated as a city in April, 1902. The development of Santa Paula has been slow and steady with an annual growth rate of about 2 percent per year. In the late teens and early 1920's, the South Mountain Oil Field began production, which resulted in increased growth during this period. Tract houses were built after World War II with some regularity to accommodate an influx of Navy families from Point Mugu, who were returning to Ventura County after the war.



In the 1940's and 50's, continued oil development kept growth steady. The 1960's saw the largest jump in population from an average of 270 people a year in 1950, to 654 in 1960. Many new subdivisions were built west of town to accommodate this boom, which slowed in the early 1970's.

Historically, downtown Santa Paula has served as the focus of business, social and cultural activity. The city is unique in its historic design, which will serve as a catalyst for revitalizing the city. The character of downtown and its compact physical nature has potential to produce a relatively high volume of daily users. The physical improvements proposed by the Downtown Improvement Plan will impact a large number of Santa Paula residents and visitors and directly affect their image of the city.

Vision Statement

Defining the future for Santa Paula is important as it moves into the 21st century. This vision statement describes a vision of the study area as recommended by the Downtown Improvement Group and adopted by the City of Santa Paula.

"The City of Santa Paula is situated in one of the last unspoiled farming valleys in Southern California. The historic downtown area of the city fits neatly within the context of this rural setting, surrounded by miles of citrus groves



and beautiful mountains to the north and south. It is situated next to the Sespe Wilderness Area, last refuge of the California Condor. Even in its rural setting, it is close to the major metropolitan areas of Southern California, yet seems to exist in a world of its own.

Downtown Santa Paula is a mix of civic, cultural, and commercial activities within an elegant setting of diverse, mostly twentieth century, architecture. It is a friendly, inviting, and somewhat nostalgic setting reminiscent of "Hometown USA". Santa Paula's downtown is visually beautiful, historically interesting, and has its own economic vitality. It has built on the past and maintains its context within the larger community.

The historic core of the downtown is Main Street between Seventh and Twelfth Streets. Tenth Street provides the visual entry to the City from Highways 126 and 150 and extends the core area to the north and south. Each point of entry to the core area is designated by green space and signature structures identifying your "arrival" in Santa Paula,

Citrus Capital of the World. Streets are lined with an interesting mix of restaurants, shops, and public spaces announcing to all that they are entering a special place worthy of spending some time.

The downtown core area is visually exciting and socially active. Streets are lined with colorful flags, sheltering awnings, inviting architectural features, flowers, green plant, street trees, attractive signs and a generally "welcome home" feeling. Public spaces are active and vital, with events and activities happening throughout the downtown area.

The central park space is the focal point of Santa Paula's weekend activities. A mariachi band entertains families while the children's theater delights children with puppet shows. An art fair attracts families, and antique shops display a variety of historic artifacts for perusing or purchasing. The farmers market displays local produce and fruit grown here in Santa Paula.

At mid-morning the train arrives at the historic Santa Paula depot bringing locals and tourists to the City for shopping, dining and entertainment. Visitors tour historic buildings, the Santa Paula Union Oil Museum, and art galleries, before dining in one of the many unique restaurants. After lunch, perhaps some shopping for that special gift for grandma, listening to a concert in the park, or attending the Santa Paula Theater Center presentation of "As You Like It" at Ebell Park. Visitors return to the depot and rent bicycles to tour other scenic and historic sites on the extensive system of trails and bike paths along the historic railroad and throughout the town.

Rounding out the day, a visit to the park along Santa Barbara Street provides an opportunity to relax in the railroad park, visit the Depot, the Mill, or grab a bite to eat at the Glen Tavern Inn across the street. This park plays an important role in the fabric of the city, giving a grand entry to the downtown area from the north, and from the railroad and County Trail System running east and west between Ventura and Santa Clarita.

The historic core area is accessible from nearby residential areas by foot, bicycle or motor vehicle, along pleasant tree-lined streets. The downtown area is bicycle and pedestrian friendly, inviting people to park the car and slow down the pace. Cross-town traffic will be redirected to Harvard Boulevard and other east-west streets and Main Street will be reclaimed for the slower pace of bikes and pedestrians, while still providing vehicle access for local residents and tourists alike. Pedestrians are good for business. Main Street accommodates only two lanes of motor traffic and has diagonal parking as a reminder of our historic past. Parking is abundant at the rear of buildings where former alleyways have been cleaned and landscaped, and parking lots have been developed to accommodate motor traffic. Access to Main Street businesses is directed through attractive and landscaped pedestrian access ways and a unified sign system along Main Street and throughout parking areas.

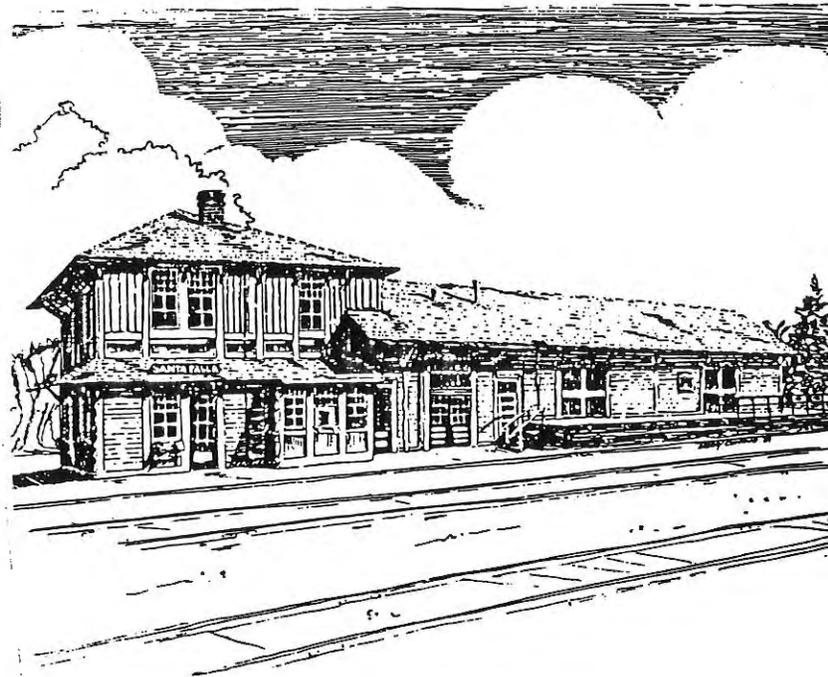
The entire downtown area is reminiscent of its former elegant architecture, with a diversity that speaks of both uniqueness and harmony. Restored facades reveal the care and craftsmanship of a bygone era. False fronts have been removed and original facades are restored with painting to accent the architecture. Maps and signs are used to lead visitors and residents alike to historic buildings. Visitors to the downtown area are drawn to key landmarks such as the Santa Paula Union Oil Museum, a citrus museum, a Santa Paula Museum of Art and History, the Santa Paula Ebell Theater Center, the Santa Paula Depot and numerous churches and fraternal halls.

The history of Santa Paula's downtown is further revealed in the tasteful design of the streetscape elements. Light posts, litter receptacles, street furniture, signs, and floral plantings unify the downtown appearance. Decorative accessories such as business canvas awnings, window signs and lighting, demonstrate the pride of the business community. Projecting signs add flavor to the historic appearance and window displays are attractively done rather than glaring and demanding. Trees and other plantings fit with other streetscape elements to unify the area and are placed to accent and enhance buildings, rather than cover them up.

The downtown area maintains a strong balance between a vital work center for locals and a place for visitors to enjoy the delights of Santa Paula. The commercial mix has been changed to concentrate on the uniqueness of Santa Paula and its resources and to successfully compete with modern shopping malls. Economic vitality has been enhanced by catering to local residents and at the same time providing extensive visitor services and activities.

The downtown area is viewed as the symbolic and social "Heart of the City". Visitors and locals alike envision Santa Paula as the center of the County and an easy destination for an experience in a true old fashioned Home Town, USA."

Assessment of Existing Conditions



An assessment of existing conditions has produced descriptive and statistical information needed and used as the foundation for the identification of design problems and opportunities and subsequently, the formulation of recommendations for the various components of the Downtown Improvement Plan designed by the Downtown Improvement Group and Technical Design Team. A summary of this information follows:

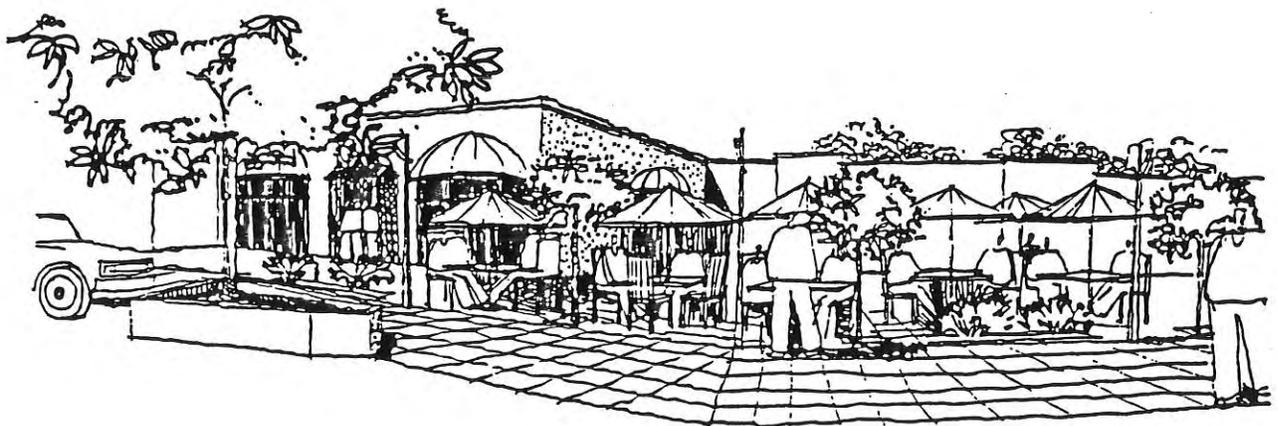
Building Conditions and Land Use

Downtown Santa Paula includes some intermixing of non-commercial land uses that are not usually considered vital or basic to commercial downtown activities. There are also clear indications of physical deterioration of structures, vacant and under utilized land parcels, parking facilities and non-complementary building uses. Under utilized parcels of land are located adjacent to historical buildings or buildings with significant architecture. Vacant lots and deteriorated buildings are located adjacent to well maintained structures. This assembly detracts from the pedestrian continuity of the streetscape and minimizes economic productivity, due to physical dislocation and disuse.

The architectural styles of buildings located along the streets of Santa Paula reflect the evolution of the City. These unique historical buildings styles are the result of a variety of influences, including, but not limited to, available building materials, architectural talent, economics, and cultural traditions.

The existing streetscape of Santa Paula, in most part, presents a semi-unified appearance, which is principally due to the building materials, accent colors, architectural styles, scales of structures and degree of maintenance. In the past, sincere attempts to modernize the storefront image of many downtown buildings has occurred. A few facade alternatives have taken place over the years with little consideration to the style of the surrounding buildings. The result of this series of uncoordinated, though well-meaning efforts, is a streetscape with a visual appearance that could be improved to reflect the historical architectural style commonly found in Santa Paula.

It is obvious that a well-coordinated effort has been made to provide and enhance amenities for the pedestrian shopper. A lack of provided or maintained landscaping is obvious throughout portions of the planning area and a confusing mix of temporary and permanent signage competes for the attention of



prospective patrons. Building signage, in some cases, tends to be haphazardly placed on the structures with the design, size, and location being inconsistent. In some cases, no attempt has been made to blend the signage in with the building style or color.

Downtown Santa Paula contains the largest concentration of unreinforced masonry buildings in the County. Over one hundred buildings are at risk of catastrophic structural failure in the event of a major earthquake. Several have been strengthened, including the Union Oil Building, but most remain in need of such work.

A variety of buildings are in need of basic maintenance or repair. This is visually apparent along the backs of structures adjacent to the alleys. Rear entrances to these businesses are neither easily recognizable, attractively maintained, nor adequately signed. In many situations, business owners are missing opportunities to have multiple access points to their place of business.

The principal zoning classification within the downtown is commercial, however, there are existing residential and service commercial uses and land use classifications within the area. A few non-conforming uses exist as well. In addition, there are also areas zoned for light manufacturing uses, located along and near the existing railroad right-of-way, that should be considered for future retail use.



Parking Conditions

The Santa Paula Zoning Ordinance requires on-site parking on a ratio according to land use. Upon review of City parking regulations and input received by the Downtown Improvement Group and Technical Design Team, it is apparent that poor circulation and parking accessibility has adversely impacted the downtown area. This is evident for the retail establishments located along Main Street between 8th and 10th Streets.

Accessible parking presents a major impediment to retail stores and other small businesses in the downtown. The lack of designated employee parking facilities places them in direct competition with customers for available spaces. Customers who are repeatedly unable to find convenient parking will shop downtown less frequently, making their purchases at other more accessible shopping centers in the City or in outlying communities.

The principal deficiency, with regard to parking, is directly related to the poor location and accessibility of existing parking areas. An unorganized arrangement of parking lot elements (i.e. driveways, business entrances, lighting) and insufficient directional/informational signage, hamper successful orientation for both the motorist and the pedestrian, and induce potentially hazardous conflicts between pedestrians and vehicles.

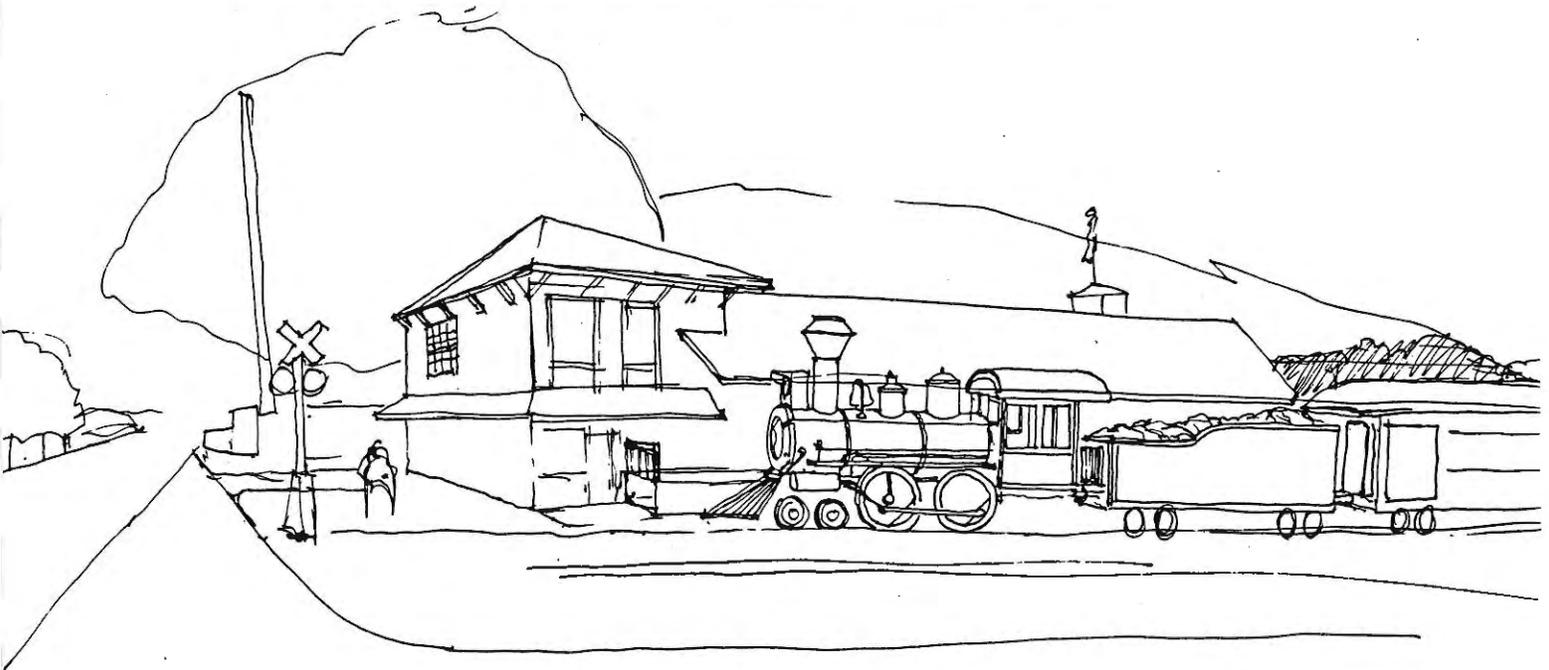
Economic Conditions

The Santa Paula downtown area, like many California downtowns, has experienced economic and physical decline. A number of buildings are vacant or under utilized. Fragmented parcels are disbursed throughout the area. Some areas are characterized by mixed uses and/or conflicting uses, which hamper economic productivity and add to the poor visual quality of the area. Property ownership is diverse and reinvestment has been limited due to multiple ownership patterns, low rents, and the high percentage of absentee landlords.

Problems most often mentioned during the Downtown Improvement Plan meetings were accessibility to existing parking facilities, traffic speeds, dilapidated appearance of the buildings, and negligent maintenance. Also noted was a lack of a wider variety of shops to serve the downtown shoppers. Vehicular congestion and parking lot problems also exist, severely limiting accessibility to the stores in the downtown area. At many of the Downtown Improvement Group meetings the need to establish visitor attractions, such as railroad exhibits and complementary museums, was discussed.

These negative, as well as positive, opportunities characterize and underscore the need for continued public and private revitalization actions. Positive efforts must

be taken to develop a more cohesive and dynamic image for the market area, if downtown Santa Paula is to remain a viable shopping district.



Summary

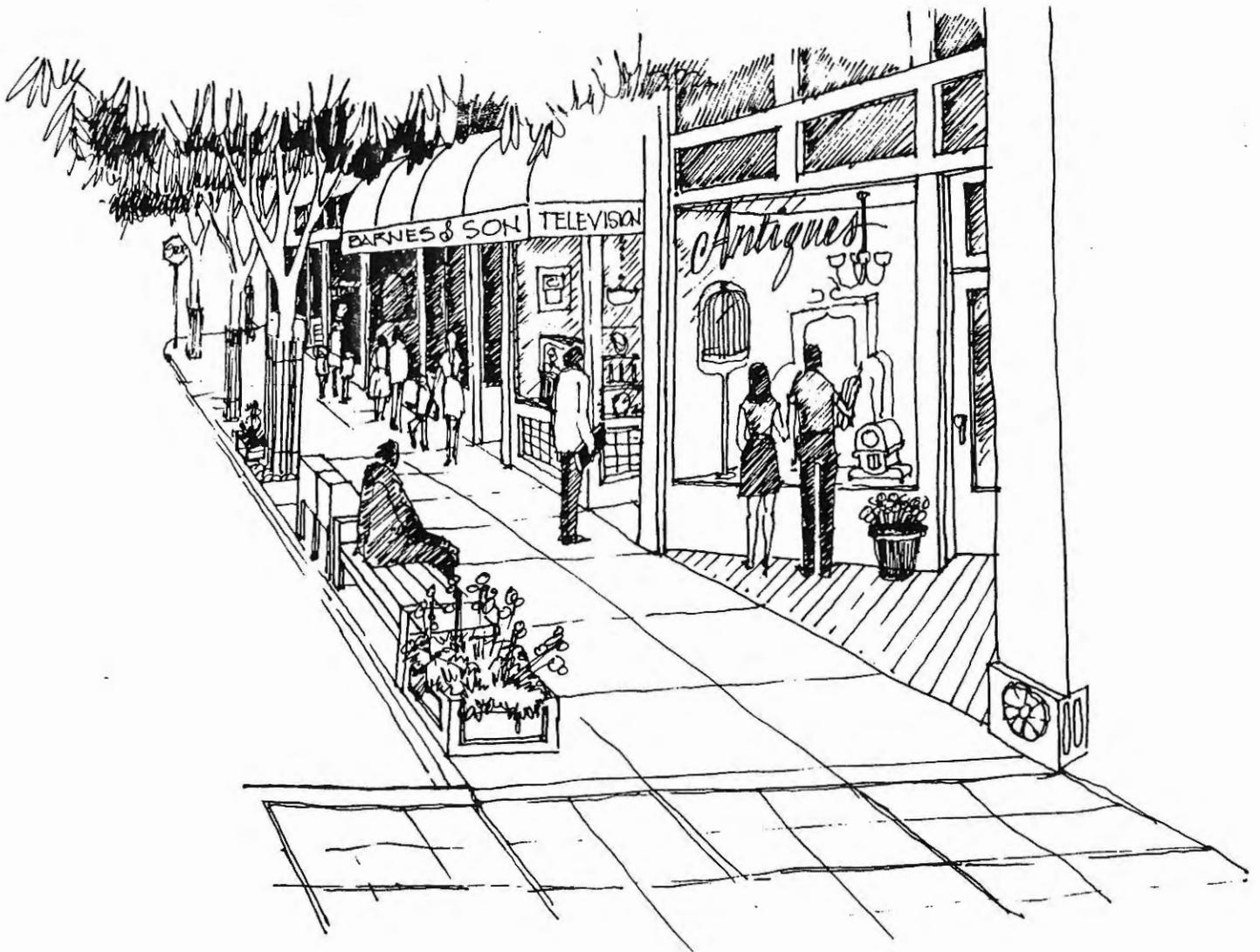
Although the downtown comprises only a small portion of the City, the central core area is most dominant and reflects Santa Paula's culture and history. However, through the years, changes have shifted much of the focus from the downtown area to strip commercial developments along heavily travelled arterial streets and other commercial centers located in outlying communities. These trends in the community are reflected in the current vacancies and decreasing business activity, which is compounded by a shortage of easily accessible parking. The result has been a loss of revenues and businesses.

The Downtown Improvement Group recognizes the detrimental effects of decline in the downtown area and the importance of maintaining it as a functional, cultural, and aesthetic center of activity. Although it is clearly recognized that there is already a well established downtown area, current conditions have led to the development of a visually and functionally weakened downtown business environment.

In view of the numerous opportunities within the downtown area, it is evident that there is substantial need for revitalization. It is well recognized that future success in implementing a revitalization program will hinge on cooperation and communication between public and private sectors of the community.

In summary, the Downtown Improvement Group and Technical Design Team have identified many issues, which they would like addressed in the implementation of the Downtown Improvement Plan. The following issues have been identified as areas of concern:

- To attract customers from surrounding areas, the downtown should emphasize a pedestrian oriented streetscape that complements the historical components of downtown Santa Paula. Many of the existing buildings have been designed and scaled for the pleasure of the pedestrian. This, in conjunction with flower pots, informational signage, displays of public art, and other such amenities can create a strong pedestrian identity for the downtown, thus giving it an individual character and commercial drawing power.



- A variety of under-maintained structures are located throughout the downtown, which impede the establishment of a continuous network of pedestrian oriented spaces. This results in the general perception of blight and visual disruption. In many cases minor repairs, such as painting and window/material replacement, could vastly improve the visual appearance of the streetscape. In more dilapidated structures, major renovation or reconstruction may be necessary.
- The existence of non-contiguous and isolated uses in the downtown area detract from the potentially close relationships, which are available in a more compact pedestrian oriented environment. The prevalence of "holes", such as under-utilized parking lots and land, buildings with no ground floor activity, and other uses which do not provide pedestrian interest, directly impede the creation of a vital pedestrian shopping atmosphere. By incorporating appropriate infill construction and streetscape treatment, this situation will be corrected and a unique "sense of place" established for downtown Santa Paula.
- There are a few scattered conflicting land uses throughout the planning area, such as service commercial and industrial uses. This fragmentation does not optimize the economic potential of tourist or retail area, nor the tax base of the City as a whole.
- The general area along Main Street, east of Ojai Street, presents a particular challenge from an urban design perspective. As it is located at the east entrance to the downtown, it provides the initial view and impression for a visitor entering from that direction. Currently, uses in that area include service commercial, light industry, manufacturing and under-utilized lots. These uses and the visual image which they project are not compatible with or representative of a vital downtown. Buffering and appropriate design and code enforcement measures need to be taken to mitigate the existing negative impressions.
- Downtown Santa Paula, as it exists today, lacks the strong visual imagery which is necessary to establish its identity as a viable tourist and general shopping destination point. Existing traffic patterns in the City do not easily identify the location of the downtown. The result is that motorists pass by without awareness of its existence. Identification signage and design elements (gateway statements, thematic styling and details, view corridors, and landscaping) can help organize the streets in a manner which will produce a pleasurable and comprehensible environment, as well

as establish a "sense of place" for the visitor. Specifically, entrance statements, both within and on the perimeter of the downtown area, should be placed at the following locations: Harvard Boulevard and 10th Street, Main Street and 7th Street, Main Street and 12th Street, 10th Street and Railroad Avenue.

- There is now an insufficient amount of accessible parking available to serve the needs of downtown merchants, customers, and employees. Opportunities do exist for the conversion of under-utilized alleyways and parcels of land near the major shopping core of Main Street between 8th and Ojai Streets.
- The transportation technology, which was available at the time downtown Santa Paula experienced its greatest growth, did not necessitate the provision of facilities for parking automobiles. There is now, within this out-dated fabric of development, an insufficient amount of convenient parking available to serve local needs as well as the possible Metro-link and tourist railroad activities. There is also an opportunity to develop an attractive greenbelt along this transportation corridor between 7th and 12th Streets. The identified open space area, located adjacent to the depot, should serve as an attractive assembly area for visitors, special events and for other retail uses.

DOWNTOWN IMPROVEMENT PLAN

Goals

The Santa Paula Downtown Improvement Plan will emphasize accomplishment of both short-term and long-term goals through a coordinated series of recommendations and proposed projects. The primary purpose of the design plan is to serve as a guide for the conservation, adaption, and enhancement of buildings and other streetscape elements contained within the Santa Paula downtown area. The Plan is based on the premise of restoring visual continuity and a rational physical form to the historic downtown. The information contained within the following pages addresses the areas of concern and responds to the wide range of opportunities available for improving the visual appearance and economics within the planning area.

Design recommendations for the Downtown Improvement Plan are outlined in the Downtown Design Recommendations (Section 2.2). It should be noted that before these recommendations can be realized, it is necessary for the Planning Commission and City Council to establish goals which are recommended herein.

The goals proposed represent what the Downtown Improvement Group as a whole wants for the downtown area, such as the type of image it would like to see projected in downtown Santa Paula.

The proposed goals, once approved by the Planning Commission, City Council, and Redevelopment Agency, will become the foundation for guiding future participants in the development of plans for the downtown area. They should provide a foundation for transforming general desires into specific objectives and establish a sound framework for making improvements in the planning area.

It is clearly recognized that upgrading and keeping downtown Santa Paula healthy will be the responsibility of both the public and private sectors. Both elements should act as catalysts in initiating actions and programs that are necessary to realize the long-term goals of the community.

Pending further refinement and expansion by the Planning Commission, City Council, and Redevelopment Agency, the following statements or goals provide a preliminary basis for decision- making and establishing priorities:

Visual Character Goals:

- Develop the retail core of the downtown by establishing an attractive pedestrian oriented shopping core which is reinforced through the use of decorative banners, lighting, street trees, street furniture, and landscaping;
- Establish an implementation program which will promote coordinated and compatible treatment of building facades and signage;
- Establish design and development guidelines which emphasize the enhancement and restoration of the original structure and maintenance of the distinctive architectural character of each building; and
- Enhance the quality of the public right-of-way and public transportation core and create a distinct identity for the downtown area as a whole; and develop a unified shopping area through infill construction and renovation.

Parking and Transportation Goals:

- Improve pedestrian and vehicular traffic flow by upgrading entry points and directional/identification signage to the downtown area;

- ▣ Develop short term programs to resolve immediate parking problems by providing additional parking, which is more accessible and convenient for downtown shoppers;
- ▣ Design a long-term parking plan, which will encourage infill development and revitalization without aggravating existing parking conditions;
- ▣ Resolve parking issues without adversely impacting existing compatible uses and new construction; and
- ▣ Provide non-motorized transportation facilities including security bike racks, bike lanes, and pedestrian-friendly streets, crossings and walkways.

Land Use/Community Development Goals:

- ▣ Encourage the use of sound design criteria in the rehabilitation of older commercial structures;
- ▣ Establish land uses within the project area that will be compatible with adjacent properties and the existing mixed use character of the streets; and
- ▣ Eliminate physical and economic blight by removing deteriorated and functionally obsolete buildings.

Downtown Design Recommendations

Concurrent with the formulation of goals the Technical Design Team designed recommendations, which focus on enhancing the image of downtown Santa Paula. Based upon input provided by the Downtown Improvement Group, a list of urban design recommendations has been prepared.

The recommendations have been merged as part of a design approach, which provides the framework within which improvements can occur. The basic premise is to provide cohesion between the individual buildings in downtown Santa Paula and suggest a program for implementing private and public improvements, which will enhance the public right-of-way and private and public buildings. Over one hundred (100) years of local development have produced a wide spectrum of building styles, however, the Downtown Improvement Plan suggests streetscape improvements, entry identification treatments, "theme" building materials and colors, and landscaping, which would create a visually unified and appealing shopping environment without destroying diversity and historical authenticity.

The Downtown Improvement Plan establishes guidelines for new construction projects, as well as those proposed for rehabilitation. These coordinated improvements will contribute to the vitality and character of downtown Santa Paula, as well as help focus and strengthen its image.

Redesign or Reorganize Existing Parking Areas and Develop New Lots that are Accessible from the Major Downtown Entry Points and are Convenient for Downtown Shoppers and Visitors

The provision of adequate parking and the redesign of existing parking facilities is crucial to the downtown's economic survival. The basic requirements of accessibility demand that motorists be able to park their vehicles in close proximity to the core area shops. It is just as important to accommodate bicycles and pedestrians.

Attention should be focused on improving the layouts of the alleyways in an effort to capitalize on their potential as inexpensive parking alternatives. The lease or purchase of land for use of parking should be explored. Include safe convenient accommodations for bicycles and pedestrians in alleyways and parking lots.

Support Expansion of Existing Uses as Well as Redevelopment and New Construction on Vacant or Under Utilized Land

The principal focus of the Santa Paula Downtown Improvement Plan/Implementation Program is the physical upgrading of the downtown. Although many of the improvements will involve landscaping and the introduction of pedestrian and visitor amenities, infill development and the expansion of uses should also be encouraged.

The overriding objective of such a program is the upgrading of key areas of the downtown and the consolidation of similar uses in order to reinforce the strength of downtown Santa Paula as a viable shopping area. Since museum, financial, government and commercial use are already present in the downtown area, the key to the attainment of this objective is to build upon the base of commercial activity, which already exists, and thus create sufficient economic potential and excitement to draw additional museum, retail, service and tourist uses to the downtown.

A consortium of local government agencies, including the Santa Paula Redevelopment Agency, has assembled a funding package to purchase the entire 32 mile Santa Paula branch railroad line extending from the main line near Route 101 in Ventura to about three miles east of Piru. A hiking and bicycle trail is proposed along with continued rail use, future commuter

rail use and other possibilities. The trail could become the backbone of a significant system of non-motorized transportation. Santa Paula will have use of the 200-foot-wide station parcel extending from Ninth Street to nearby 12th Street.

The existence of the railroad corridor and future purchase of said property presents an opportunity to develop the existing right-of-way for an attractive well landscaped activity center. This open space corridor will serve as an attractive entrance way and focal point for downtown, which could accommodate rail related museum facilities and visitor activities.

Improve the Streetscape through the Preservation, Restoration, and Enhancement of the Existing Structures in a Manner which is Sympathetic to the Original Character of the Building and to the Streetscape

The pleasing quality of old commercial main streets relies heavily on the compatibility of texture, colors, rhythm, materials, and proportions of the buildings. It is this quality of visual coherence which the following design standards and guidelines will attempt to resolve with solutions to problems which generally occur when renovation or restoration takes place.

Many of the buildings in the Santa Paula downtown have been altered over the years. Quite often these alterations have concealed the architectural qualities of the original facades, including the boarding up of former window openings, the replacement of earlier lettering with oversized or poorly painted signage, and the resurfacing of the original materials with stucco or wood. New design elements, colors and building materials should be used to enhance and/or be consistent with the design of the original structure.

In cases where expense or adaptability make restoration infeasible, changes should be made in a complementary style, using materials which reflect the character of the existing streetscape. Consideration must also be given to the overall stylistic character of the building to be rehabilitated. Respect should be given to the original design of the structure rather than introducing a new, uncomplimentary or incompatible design style.

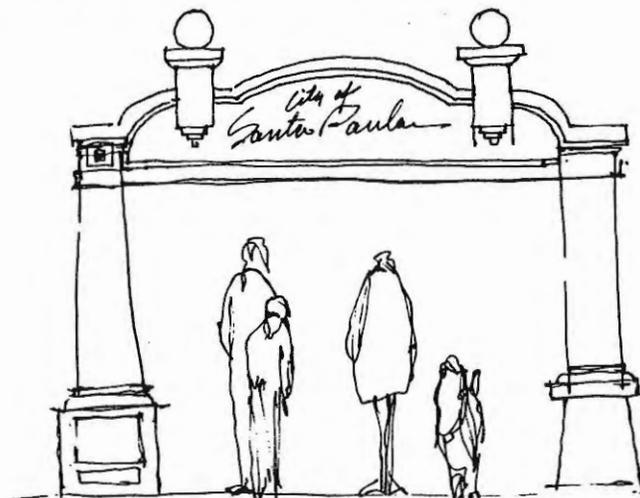
New construction should be done with a great deal of sensitivity, reflecting the forms and architectural details of the older adjacent buildings, yet with simpler contemporary design. A well designed new building, which is compatible with the surrounding structures in terms of scale, color, texture, form and mass is usually better than a fake antique.

Enhance the Downtown Area through Facade, Signage, and other Improvements, which follow Recommended Design Standards and Architectural Guidelines

The primary objective of the Downtown Improvement Plan should be the upgrading of the overall area. Buildings in the downtown area constitute many styles, themes and colors. Implementation strategies should concentrate on rehabilitation, which minimizes the visual disharmony and encourages streetscape uniformity and successful linkages between buildings.

It has been suggested that an "enhancement approach" or preservation of the original building be used as a guideline for any new or rehabilitative work to be undertaken in the downtown area. Use of the enclosed color palette, list of building materials, and signage guidelines should be encouraged in an effort to promote uniformity of design and an aesthetically pleasing environment. In conjunction with these projects, other improvements should include new parking facilities, landscaping, pedestrian lighting, and appropriately related street furnishings. This approach will place more emphasis on pedestrian spaces.

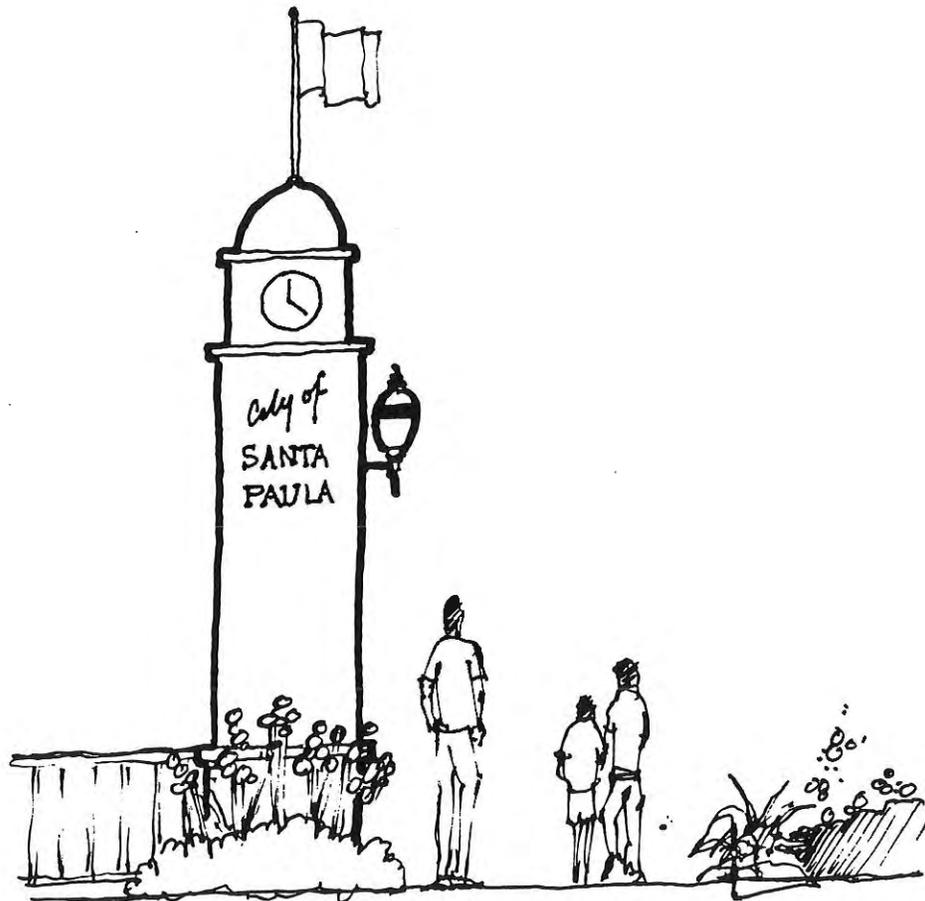
The purpose of the Downtown Improvement Plan is to create a comprehensive pedestrian network throughout the area where walking is emphasized as the primary mode of circulation. Landscape along Main Street will create a pedestrian scale streetscape and help transform this area from a vehicular thoroughfare to a more pronounced shopping district. This mixed system of design components will foster more pedestrian space at principal intersections and special treatment areas. The development of special treatment areas should provide cohesion among individual projects linked by the proposed design elements and should produce a continuous network of pedestrian spaces.



Identify major entry points and intersections through special treatment such as signage, flags and landscaping materials

Entry points should increase clarity of access and serve to identify the location of the downtown area. Although these gateways will not be the only means of access into the downtown, visitors will be directed to them through use of local arterial roads. Highly visible and easily identifiable entry ways are proposed for four intersections as identified on the Downtown Improvement Plan. They will be clearly delineated in an effort to create a distinct and significant identity.

Entryway or gateway signage is important to establish a clear Project Area identity. The use of an entryway sign can serve to emphasize an arrival point or reinforce the image of the area as a unified retail district. Gateway signage can be custom made or prefabricated metal and brick and include such information as the downtown name and the City of Santa Paula logo. Additional design elements to accompany street graphics might include flower beds or landscaping and planter treatments.



Provide a composition of related street furniture such as benches, street lights, and trash receptacles, which will support the pedestrian orientation of downtown Santa Paula

The introduction of pedestrian oriented design elements is necessary for the production of a strong retail area. Street furnishings which are design related, such as decorative lighting fixtures, trash receptacles, and benches can create a harmonious streetscape and lend a more human scale to this pedestrian oriented environment. Tastefully designed street furniture can serve to unify diverse architectural elements, as well as define the pattern of the streetscape.

Provide for the Undergroundings of Overhead Utilities

The undergrounding of overhead utilities is suggested as a means of removing negative visual elements from the downtown area and increasing the ease of pedestrian circulation within the area. Capital improvement facilities should be maintained at a high level to reinforce the overall revitalization effort.

Downtown Projects and Programs

Building Restoration Program

The buildings are the most important asset in the downtown area. Their restoration, preservation and rehabilitation is therefore the most important part of the project. Cosmetic restoration is not enough. Recent events have shown that seismic rehabilitation of unreinforced masonry buildings is critical to the long-term health of downtown. None of this work can be done without a financing program that will make the improvements affordable to the property owners and merchants.

The key components of the Building Restoration Program are:

- Facade restoration, front and rear including removal of metal or wood siding, removal of inappropriate signs and add-ons, uncovering and restoration of architectural features, replacement of damaged materials, appropriate color selection and repainting.
- Historic preservation including investigation of original design from old photos and architectural features, restoration of existing elements, appropriate selection of new elements, possible historic district.

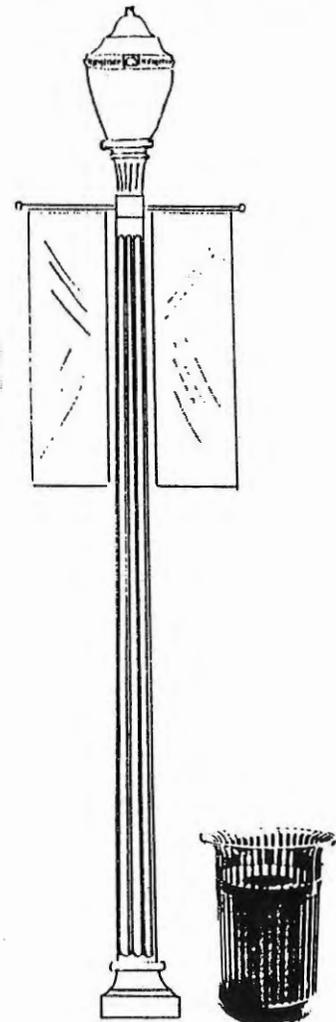
- Seismic rehabilitation including engineering investigation, structural bracing, improved connections, and conformance with rehabilitation ordinance.
- Seed money for engineering and architecture including city-sponsored investigations, cost estimates and drawings.
- Long-term financing/refinancing including low-interest rates, improvement financing and consolidation of existing financing at better interest rates and payment levels.
- Architectural guidelines including historic context, material and color selection, style, density, parking, and mixed uses.

Streetscape Project

The streetscape should complement the buildings, not compete with them. Extensive use of colored concrete pavers, pedestrian malls and other modern concepts are out of character with the period look of the downtown area. The proposed project would instead seek to replace the more modern features with ones more in keeping with the early half of the century. One highlight of the project would be new street lighting equipped with cross arms for frequent use of colorful banners. Poles similar to those used in Santa Paula seventy years ago are being manufactured to this day in nearby Fillmore.

The key components of the Streetscape Project are:

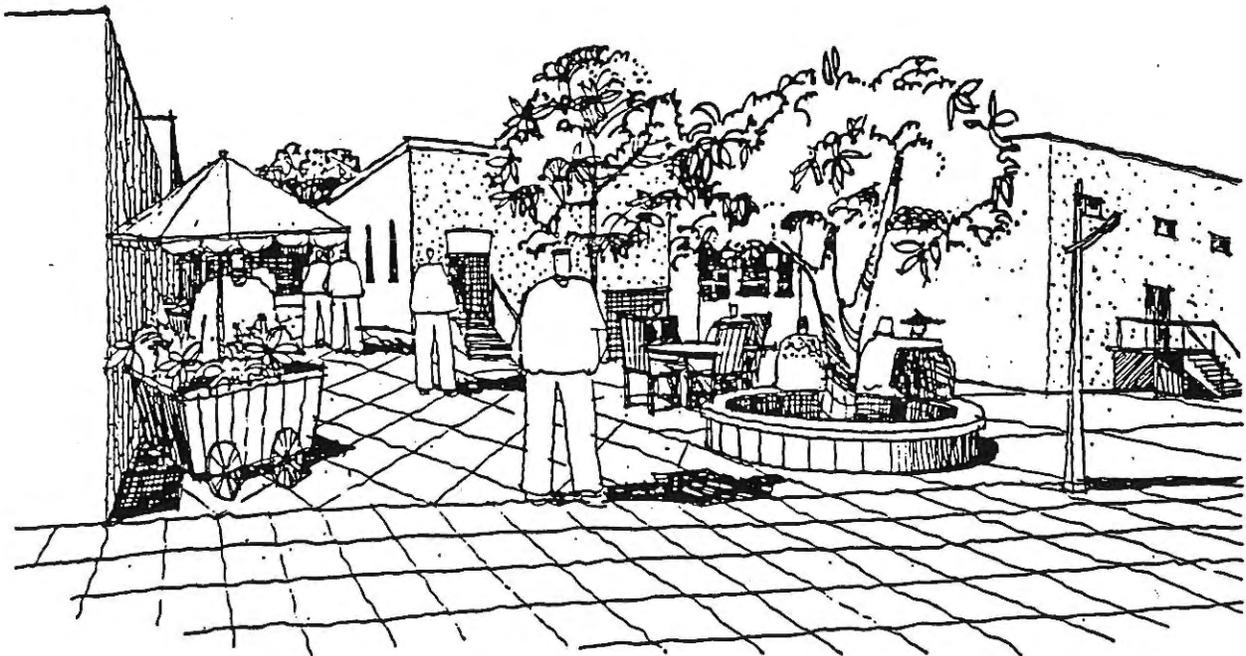
- Historic street lights with banners, including traditional design poles and lamps, cross arms for double banners, double lamp fixtures at key entrance locations, provision for auxiliary electricity supply for lighted decorations and for a sound system.
- Street furniture including wrought iron benches and litter receptacles, coordinated flower pots, appropriate designs and colors for other necessities such as bicycle racks and newspaper racks.



- Signage including decorative signs at important locations such as museums, limited use of traffic signs, sign poles painted to match street furniture, appropriate entry signs and freeway directional signs.
- Tree control or replacement including a survey of all trees to identify troublesome locations, chemical control of berries, phased replacement of the worst offenders, if necessary, more variety in tree species.
- Spot landscaping at key locations including planter boxes at mid-block crosswalks and at the Odd Fellows building, additional landscaping at entry locations and other selected sites.
- Tenth Street widening eventually including the east side of the street between Harvard Boulevard and Main Street, but initially only the west side south of the Police Station.

Green Street Plaza and Park Development Project

A unique opportunity exists to create a landscaped community plaza out of existing alleys and parking lots at Green Street. The site would provide an outdoor marketplace and park that would be used for events such as a farmers market, craft fairs, concerts, festivals and shows. Events could be held frequently because street closures would not be needed. Major events would continue to use Main Street itself. The downtown area should be bounded on all four sides by public parks. These will serve as focal points for events and activities and will



serve the needs of the downtown residents. A new park at the east end of the project area, along with the railroad improvements will complete the ring of parks around downtown.

The key components of the Green Street Plaza and Park Development Project include:

- Green Street accessway including a landscaped walkway at the existing alley location, possible shops opening onto the walkway, an ornate entry from Main Street.
- Green Street Plaza including shade trees, landscaping, textured pavers, a fountain, restrooms and utility connections for events, continued day-to-day access to existing buildings and parking.
- New park near Eleventh Street and Main Street.
- Expansion of Veterans Memorial Park to include the street area between the park and Civic Center.
- Civic Center grounds improvements including a pedestrian plaza between the major buildings.

Access and Circulation Project

Convenient vehicular access, parking, bicycle and pedestrian circulation is essential to the long-term success of the downtown area. The existing arrangement of parking lots requires pedestrians to walk in back alleys or long distances to stores and will limit the potential of the entire downtown project. Changes in parking and pedestrian access can create a more inviting setting, a sense of activity and a more intimate business and shopping atmosphere. As projects on the railroad right-of-way come on line and tourist business increases, improved access will be needed between the railroad, downtown and the airport.

Pedestrian sidewalks will need improved lighting and amenities such as benches, drinking fountains and litter receptacles. Eventually, tram service could be warranted.

The key components of the Access and Circulation Projects are:

- Parking lot rehabilitation, expansion, and landscaping including repaving where needed, planting of additional trees and planter

areas, appropriate signs, replacement of deteriorated improvements and added bicycle parking facilities.

- Angle parking reestablished on Main Street from Eighth Street east to Tenth or possibly Eleventh Street.
- Alley rehabilitation including repaving, improved signs and lighting, improved trash storage areas and some tree plantings where room permits.
- Pedestrian accessways including the Green Street project discussed above, one existing access from the north alley parking area and two new accessways from the expanded library parking area and south alley parking area, with lighting, landscaping and amenities.
- Access to railroad and airport including signed pedestrian routes with street furniture, rest areas, drinking fountains, lighting and amenities.
- Tram service established on an appropriate schedule using an attractive 1920's vintage bus or similar open design vehicle with a period look.

Commercial Redevelopment Program

The possibilities of this program are enormous. New museums and attractions could draw from the local population and visitors alike. Underutilized sites presently form "gaps" in the building frontage that could be filled in with new buildings. New development on south Tenth Street would make the entry into the downtown area more inviting and development on Railroad Avenue would serve neighborhood needs and the expanded use of the right-of-way that is discussed below.

The key components of the Commercial Redevelopment Program are:

- Museums and attractions including such ideas as a citrus museum, an aviation museum, and a Santa Paula art museum, restaurants and outdoor dining areas, shops and boutiques and other ideas that would attract visitors.
- Infill vacant sites including new buildings in the "gaps" with size, style, setbacks and parking similar to the existing buildings.

- South Tenth Street redevelopment including new commercial development on both sides of the street between Harvard Boulevard and Main Street, relocation of residential uses to other areas or into mixed-use facilities.
- Railroad Avenue redevelopment including new commercial development on the north side of the railroad property, such as expansion of existing businesses or new neighborhood commercial activities and eventual commercial development to serve the activities on the railroad right-of-way.
- Project assistance including various activities of the Redevelopment Agency such as assistance with property acquisition, assistance with financing and assistance with housing or relocation and assistance with promotions.

Railroad Right-of-Way Improvement Project

Federal and local funding is now in place for the purchase of the entire 32-mile-long Santa Paula Branch railroad right-of-way. The Cities of Ventura, Santa Paula and Fillmore along with Ventura County will jointly own the line from its junction with the mainline in Ventura to several miles east of Piru. Federal funding is also in place for design of a hiking and bicycle trail. The right-of-way makes a number of projects possible.



The key components of the Railroad Right-of-Way Project are:

- Public ownership of the entire right-of-way including the 200-foot-wide station parcel in the downtown area between Ninth Street and Twelfth Street.
- Park and trail including a segment of the paved hiking and bicycle trail that will run the entire length of the right-of-way, connecting to trails on the coast and inland and a landscaped greenbelt or linear park through town and on the wider right-of-way section in the downtown area.
- Tourist train station and siding between Eighth Street and Tenth Street to function as the western terminal of the operations of a recreation railroad.
- Possible street closures of Ninth Street and Mill Street where they cross the right-of-way to facilitate the parking of trains in the area and to expand the park and commercial land.
- Commercial development along the north side of the right-of-way fronting on Railroad Avenue.
- Space for future Metrolink station on the north side of the right-of-way just east of Tenth Street reserved for the parking lot and platform that transportation studies have indicated will be needed.

DESIGN STANDARDS & ARCHITECTURAL RECOMMENDATIONS

Introduction

This section provides a review of design recommendations that serve as a basis or foundation for the improvement of the downtown area. These guidelines are intended to provide practical information for the improvement and maintenance of the downtown area.

In order to enhance the image of downtown, suggested standards for public improvements and private efforts by merchants and property owners will need to be followed. This is to ensure the development of a unified streetscape, which will contribute to a more pedestrian oriented shopping area. These guidelines provide a basis for decisions regarding appropriate design and materials used for rehabilitation and renovations, as well as streetscape amenities, which will complement the unique character of downtown Santa Paula. Recognizing that

selective replacement of existing buildings may be necessary at some point in the future, guidelines for new construction are also included.

The illustration examples below are intended to be used as images which communicate ideas and are not recommendations for specific solutions. They are suggestions for public and private improvements, which will encourage pedestrian circulation and create a warm and inviting downtown shopping atmosphere.

Facade and Streetscape Appearance

As related to the Downtown Improvement Plan, streetscape is defined as all that which is visible from the street, which gives an appearance and character. This includes building facades, landscaping, paving, utility poles, traffic signage, commercial signs and other miscellaneous objects.

One of the primary goals identified during the planning process was to develop a unique streetscape, which would provide a clear identity to downtown visitors. Because building facades and their storefronts are the most important elements of the streetscape, every attempt should be made to integrate their design with the current themes, fabric, and texture of development, in order to more effectively increase the overall impact. It is further recommended that accompanying street furniture be designed to aesthetically and functionally complete the image of downtown Santa Paula.

The overall identity for the City of Santa Paula will be most influenced by its retail activity, generally bounded by Yale Street, 7th Street, Railroad Avenue, and Twelfth Street. The architectural styles of buildings located along the major streets of the downtown are representative of the evaluation of the City. The original fabric of the downtown can be seen throughout the planning area.



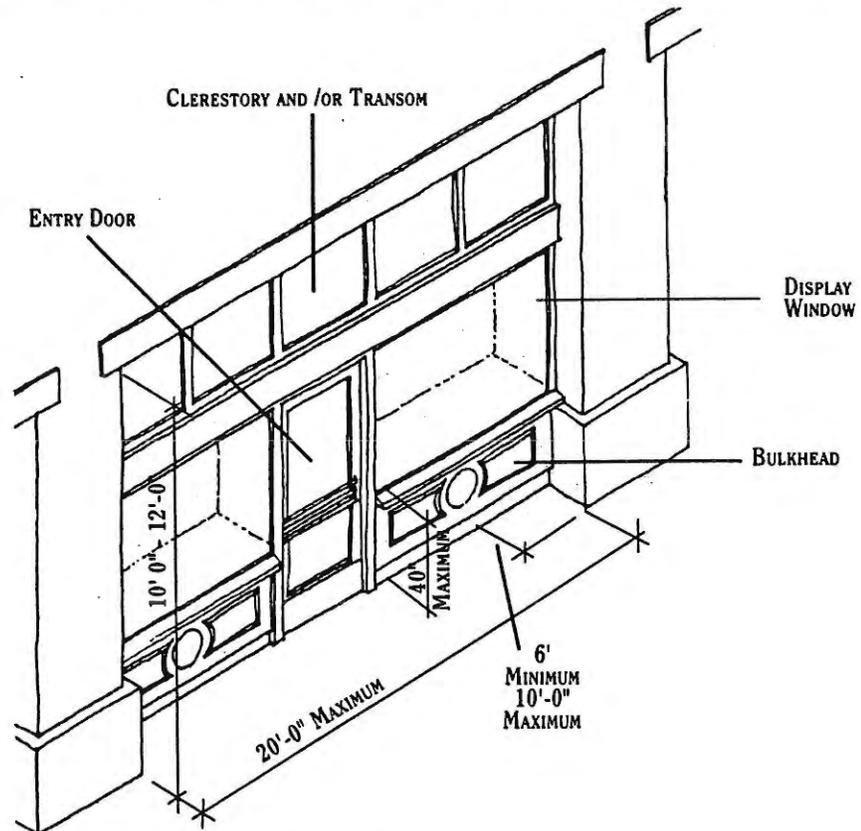
Not all main streets have buildings of a single style and even those that do were not designed with visual continuity in mind. Nevertheless, nineteenth and early twentieth century limitations on building materials created a fairly uniform vocabulary of facade designs. The typical buildings, which are found throughout downtown Santa Paula, are one or two story buildings with cornice details. Other

Introduce Thematically Related Street Furniture such as Street Lights, Trash Receptacles, Flower Pots, and Benches, which will Reinforce the Pedestrian orientation of Downtown Santa Paula

Pedestrian amenities and street furnishings such as street lights, landscaping, and benches should be used to connect the public walkways and unify diverse physical elements. Even though the visually dominant building facade and the streetwall are primarily responsible in establishing the identity of Downtown Santa Paula, thematically related design details found in street furniture, lighting standards, and other amenities will complete this image for the visitor. Street furnishings, like buildings and spaces, are part of the whole which creates the image of the City. By extending the building materials and design details of the predominant architectural style into other streetscape elements, a cohesive "sense of place" for Santa Paula will be established.

New Facade Design must be Accomplished with Care and Sensitivity, Reflecting the Form and Proportion of Older Adjacent Buildings

A few buildings aligning the streets in Downtown Santa Paula have experienced a variety of rehabilitation schemes that conceal rather than reveal the original architectural qualities of the existing facades. They are

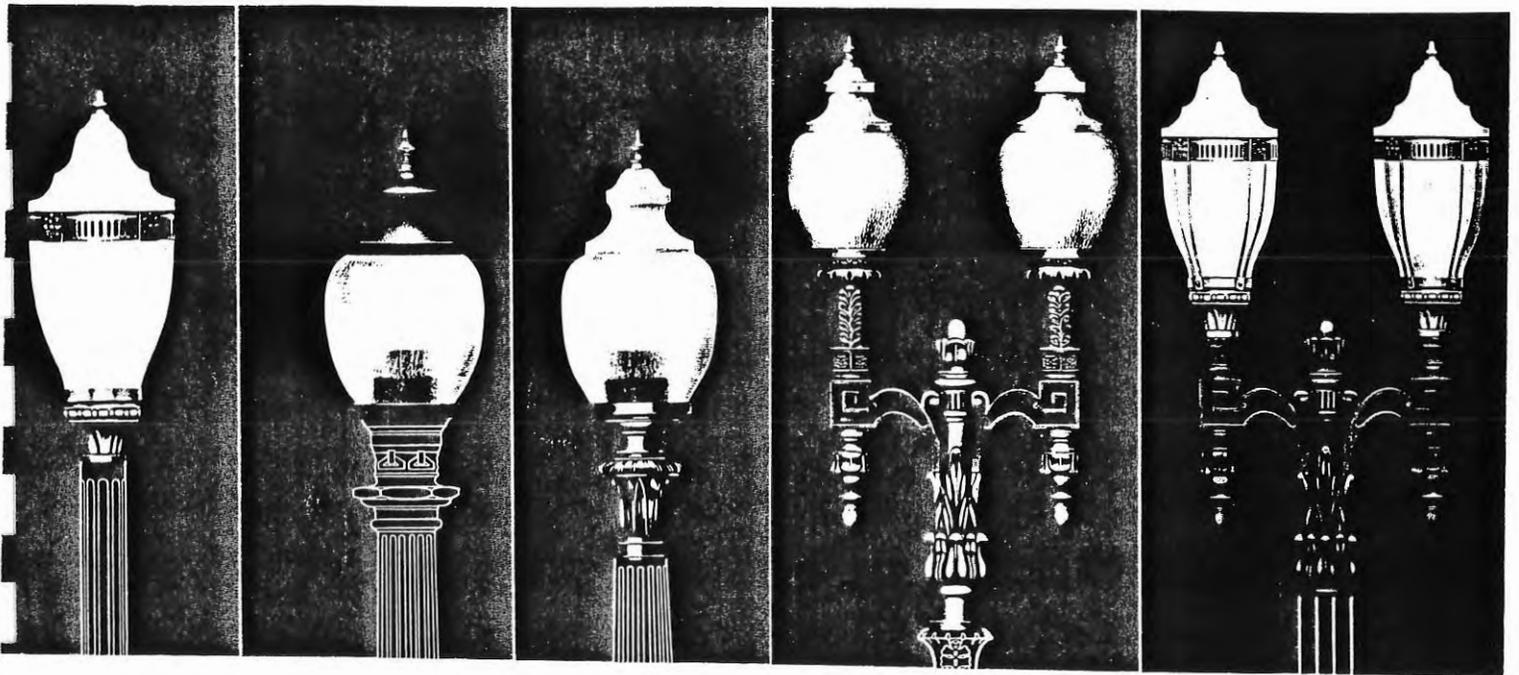


architectural elements include brick facades, recessed entries, awnings, and tile and glass storefronts. Common architectural elements include, but are not limited to, rough textured stone or brick masonry details and columns.

Today, due to the available range of styles, materials and technology, the terms "rehabilitation" and "new construction" are much broader. The purpose of the Design Standards and Architectural Recommendations is to show how the continuity of the street can be reinforced by working within a vocabulary developed from existing structures. The recommended guidelines below have been developed in order to mitigate some of the negative visual impacts associated with poor facade rehabilitation and new building design, including the patchwork of architectural details and improvements resulting from inconsistent alterations and insensitive new construction, which is unsympathetic to the character of the existing streetscape.

These guidelines are based on a multitude of design sensitive criteria, including the qualities of the surrounding building, the historic character of the area, the original style of the building, and the desired vision for the City's future.

The continuity of the shopping streets can be reinforced by working with the existing physical characteristics. The "enhancement" approach and the design standards, which follow, take into consideration the overall character of the street and seek to maintain its continuity by retaining the common components, which are integral to the historical character of downtown Santa Paula. These standards should address the above areas of concern by strengthening the downtown's unique pedestrian orientation and contributing to the exceptional character of the community.



poorly related to the traditional streetscape due to the fact that the improvements have been unsympathetic to the original character of the structure. Insensitive design or choice of building materials which are out of context with the city's character, have caused some visual disruption to the overall unity of the streetscape.

Based on input from the Technical Design Team and Downtown Improvement Group, it has been recognized that sensitive design which is based on the original architecture of the building to be improved is well suited to Santa Paula's rich architectural heritage. Rather than a "patch-up" approach to renovation, consisting of oversized signage and garish color combinations, a simpler "enhancement" method that respects the character of the existing buildings is recommended. Attention must be paid to the preservation of the architectural details and elements which lend a unique character to the City's existing buildings. A particular sign, door, ornament, or window may have a certain quality worth preserving and its removal would result in a loss of identity to the building and subsequently to the City.

Additions to the basic facade should serve to emphasize the lines and shapes of the facade themselves. Awnings, for example, can add character and color to a building while reinforcing the visual separation of the upper and lower levels.

There are times when a shopowner may be confronted with a situation where original or early storefronts are too dilapidated or expensive to restore to their original status. Rather than alter the architectural and historical character of the building and its relationship with the street setting, compatible materials, scale and design should be utilized. Local loans/grants may help to retain the desired existing design style of such structures.

Good Contemporary Design should be Encouraged for New Building Construction within the Downtown Area

A well designed new building which is compatible with the old in its scale and use of materials is generally better than a fake antique. New construction should reflect the forms and proportions of the older adjacent buildings, yet with simpler contemporary styling.

Discretionary review of proposed new construction is needed to determine which projects will have a negative impact on the overall image of the Downtown and which will make a positive contribution. If introducing modern additions or mixing old and new elements on the outside of a

building, make sure that its character is not spoiled in the process and if possible, consult with a professional designer with experience in such work.

Proportion, Rhythm and Scale

New construction should fit in harmoniously with existing buildings and neither dominate nor compete with them in color, scale, material, height or width. New buildings should be articulated in such a manner as to relate to the scale of the surrounding structures. Their design should be guided by the proposed use, as well as by the height, bulk, and character of neighboring structures.

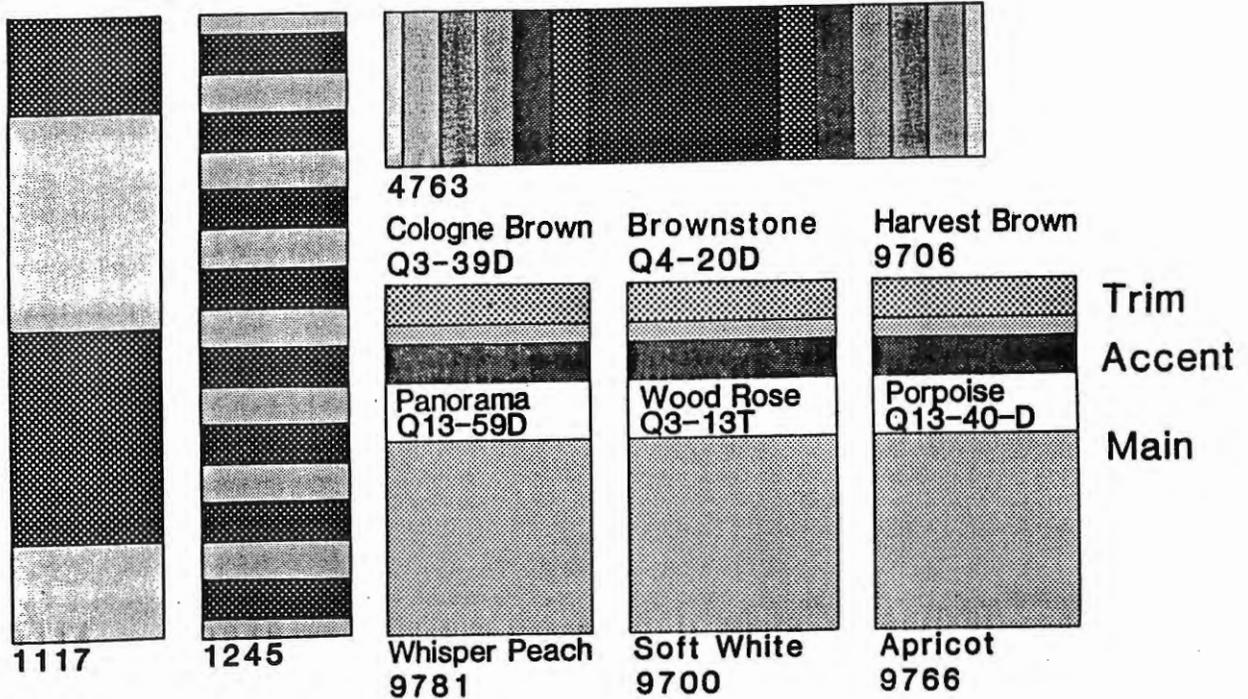
The original size of lot frontages are expressed in a few of the older buildings, which line Downtown streets. This repetitive pattern of buildings is further emphasized in specific structures by the even placement of windows or pilasters which articulate the facade. Buildings should be rehabilitated to accommodate the existing character, rhythm, and pattern of development which has historically evolved. The scale of building components such as windows and doors should be compatible with the surrounding structures. The massing of buildings should remain consistent with the other structures lining the streetscape.

Colors and Materials

Building materials in Downtown Santa Paula vary according to the age and style of the buildings. Brick masonry, plastered brick, cast stone, wood, and exposed concrete and plaster, are the most prevalent building materials used in Downtown structures. Presently there is no uniform color scheme, nor is attention given to the distinctive architectural details of the existing buildings, which might express the individuality of Downtown Santa Paula.

Coordination with neighboring facades and selection of harmonious base and accent colors is highly recommended to promote visual continuity and a special sense of identity for Downtown Santa Paula. Attempts should be made to develop building treatment designs to assure coordination of color, design, materials, and renovation methods, in hopes of achieving the greatest visual impact.

Based upon input from the Technical Design Team and the Downtown Improvement Group, it has been suggested that colors which are appropriate to the original architectural style of building be utilized Downtown. Due to the wide variety of historic styles in Downtown, there are a multitude of colors and tones to choose from. However, a general guideline to follow when selecting a color



scheme is to avoid the use of bold colors, such as pure reds and blues, other than for accent features. Such colors were not used historically due to the limitations on pigments available. In addition, due to their susceptibility to fading, they will not hold up well in extreme weather conditions. Subdued and muted tones are recommended to achieve the desired effect. The palate display gives a range of possible color combinations which would be appropriate for the Downtown.

Suggested Materials

The following list of building materials should be used in new construction and major rehabilitation projects in order to integrate them into the existing fabric of architecture in Downtown Santa Paula. Although the use of other materials is not prohibited, as indeed their use can add to the visual interest, each building should incorporate a substantial portion of the recommended materials, colors and/or other design elements (scale, proportion, rhythm, pattern, etc.) in order to assure its contextual fit within the overall character of the Downtown.

- Recommended building materials for the facades of commercial buildings along major streets are stucco, plaster, unpainted brick, and exposed slumpstone.

- Natural unpainted red brick is highly recommended for storefront renovations. However, a less expensive alternative such as brick tile can be substituted to add color and contrast to the streetscape.
- Generally, new and remodeled buildings should not emphasize modern construction materials such as stainless steel and highly tinted mirror and glass. When tastefully designed, clear glass display windows are an important eye-catching element, which can serve as a form of signage to communicate the products and services available inside.
- Window and door frames should be consistent throughout the Downtown. Materials that will achieve a more traditional look are recommended for use.
- The use of compatible materials is also suggested as an aesthetic detail to enhance the existing architecture.
- Accent building and paving materials include tile and quarry stone. Moldings composed of wood can also provide desired detail to doors, windows and cornices.
- Non-metal or canvas awnings can add color and shade to a building facade, as well as provide an area for signage and additional graphics. Cloth awnings, other than those with vinyl finishes, should be discouraged. Regardless of the type and materials used, awnings should fit into the storefront design scheme of color, pattern, shape, size, and location. The colors chosen should complement the building colors and be coordinated among different shopkeepers.

Signage Standards

Signs are a necessary part of a successful commercial area. Most downtown businesses require some signage to help customers locate their shop. Well designed signs are more than just a necessity, however, they add to the overall charm and character of the area. They add vibrance and life to an otherwise empty setting. They tell regular customers and first-time visitors alike that the downtown area is open and that the customers are welcome. "Pedestrian scale" signs are particularly important in this regard.

Poor signs, on the other hand, can convey a message of blight and decay. They can send a warning to shoppers to beware that the business is poorly run. This

message not only applies to the particular business, but to the entire area. The collective quality of the signs relates directly to the overall impression of the area.

As with all things, signs must compromise a number of conflicting goals. A sign's aesthetic qualities, its message, and its "eye catching" ability frequently work at cross purposes. The Santa Paula sign ordinance addresses many of these problems. It prohibits animated signs, flashing lights and many other problems. It also sets size limits and establishes review procedures.

State law has made it extremely difficult to amend sign ordinances. Any change triggers a need for an expensive, full-time enforcement program. Therefore, the following suggestions are intended as guidance as to the types of signs to be encouraged and the types to be discouraged. Guidelines should be written for signs in the downtown area.

Foremost, among all considerations, signs should be architecturally compatible with the buildings they are attached to. There is no single style, size or type that is right for all buildings. Each sign must be designed to fit the size, style and character of the building it is on. It should complement the building rather than overwhelm it. It should work with architectural elements rather than conceal them. Its colors and materials should be carefully considered to be in character with the building and the surrounding neighborhood and to be sufficiently durable to assure that the quality will last form year to year.

Several types of signs should be particularly encouraged. These include window signs, small projecting signs and signs on the end skirts or undersides of awnings. These are intended to help pedestrians identify businesses. Large wall signs are frequently too high up the building face to be effective for pedestrians at street level. Traditional sign styles can add to the historic character of a building. These might include such things as neon signs, barber poles and even brightly lighted theater marquees. The important consideration is that the sign be compatible with the architecture and period of the building.

Several types of signs should not ordinarily be used in the downtown area, even though they may be allowable under the sign ordinance. Foremost among these are large contemporary internally illuminated signs and individually lighted letters. Only a few of these signs presently exist in the downtown area and those that do are conspicuously out of place. Billboards, balloon signs, large pole signs and other mass advertising techniques have no place in the downtown context. Signs above the bottoms of second story windows are also out of place as are sandwich signs on the sidewalk and "temporary" business signs that have been up too long. Inappropriate signs, broken signs, old sign brackets or signs that are deteriorated or faded should be removed or replaced. This would be welcome at any time, but

would be particularly appropriate as part of the restoration of the building's facade. Billboards should be phased out and removed.



Finally, new signs should have a neat, professional appearance. Sloppy signs with ill formed letters, poorly drawn graphics, substandard materials and an obvious "homemade" look can contribute to a run down, blighted atmosphere that drives away residents and visitors alike.

Landscaping and Open Spaces

Visual interest and effective buffering may be created by using a variety of plantings, and by the spacing and grouping of street vegetation. Transition between uses can be maximized through proper screening techniques, which use various combinations of plants, hardscape and open space. Plant covered walls or free standing shrubs planted in groups can be used to create visual barriers.

Careful consideration should be paid to creating a warm, inviting and attractive atmosphere, which is enhanced by the creative use of vegetation rather than an indeterminate use of foliage. Sufficient attention should be given to the preliminary site analysis, maintenance availability, and other predeterminations, which are essential to the survival of the plants before design decisions are made and the landscaping is implemented.

Success with plantings under urban conditions is dependent upon a variety of criteria, which include the following:

Visual Appearance: Height, color, and density of foliage and individual characteristics of particular picturesque trees.

Suitability: Tolerance to climatic condition, temperature variations, fumes, air pollution, soil structure, and exposure.

Maintenance: The amount of public or private landscaping maintenance which is available.

Availability of Space: How much space will the plantings cover and how much is available on the street? Given the space requirements, what types of street foliage is appropriate?

Growth Rate: What is the most effective height of the trees and shrubs needed in Downtown Santa Paula and how will this effect their maintenance?

Appropriate trees and shrubs should be recommended for use in Downtown Santa Paula by a qualified landscape architect. The vegetation selection should consider conditions in Downtown Santa Paula, as well as the above mentioned criteria.

Street Furnishings and other Pedestrian Amenities

Streetscape enhancement should encourage pedestrian activity by creating a warm and inviting sidewalk environment. A successful streetscape system of well placed and appropriately designed street furniture, such as lighting equipment, flags, plant containers, outdoor seating, and decorative trash receptacles can serve to unify the design of storefronts and enhance the proposed design theme on the established streets and walkways.

These design elements will not only enliven the streets, but will also establish specific design features that will contribute to the special identity and character of downtown Santa Paula. Attention should be paid to the selection, siting, and layout of different street furnishings in order to develop a system of physical elements, which are visual assets as opposed to a mass of confusing visual clutter.

Simplicity, avoidance of clutter, and the components of the other elements in the streetscape design scheme, are guidelines to use when selecting complementary street furnishings. The following list is by no means exhaustive; however it offers

suggestions as to other types of pedestrian amenities, which can be integrated with the physical elements which will be discussed.

Sidewalk and Intersection Improvements

In keeping with the simplicity of the overall streetscape and to further enhance the building facades as the dominant design element, existing concrete sidewalks should remain if deemed to be in safe and serviceable condition. The use of specialty paving at sidewalks should be limited to the "focal point" intersections identified on the Downtown Improvement Plan. A pedestrian's view of a clearly defined intersection should not have to rely on the use of complex paving patterns and over-designed sidewalk improvements.

Where specialty paving materials are used, it is recommended that materials are designed in common with the historic style of Santa Paula. For example, original stone curbs can still be found on East Main Street, west of Seventh Street. Because of budgetary restraints, the use of actual Sespe Stone that matches the original curbs is prohibitive. But with selective use at focal point intersections, curb stones that match the original curb's color and texture might serve to celebrate the historic authenticity of downtown Santa Paula while providing a focal point detail that is scaled to the pedestrian.

The intent of new intersection improvements should be to reduce the speed of vehicles and provide a safe zone for pedestrians. Use of special paving patterns and paved crosswalks in the intersections should be discouraged and consideration should be given to a simple "raised" paving section that controls vehicle speed while also defining the pedestrian crosswalk.

Street Lighting

Street lighting not only illuminates the downtown core area at night, but is a contributing factor to the overall appearance of the retail district by day. Effective street lighting hardware, such as flags, can help to create visual rhythms with other elements of the street, including street trees and paving materials. However, where the lighting is not designed to fit in harmoniously - in terms of scale, siting, and design - it will only add to the visual disorder and create a confused identity for downtown Santa Paula.

Detailed attention should also be paid to the choice of lighting standards. The amount of illumination needed in any particular outdoor area in the city depends on the nature of tasks occurring in the area and the degree of required visibility. Warmth of light and a personalized atmosphere are essential elements of a successful nighttime commercial business district. Street lights similar to those used in Santa Paula seventy years ago are available today with modern optical

qualities incorporated into the old styles. One manufacturer is located only a few miles away in Fillmore.

IMPLEMENTATION STRATEGY

General Guidelines

The major focus of the Santa Paula Downtown Improvement Plan has been on the identification of problems and opportunities in the downtown area and the establishment of design guidelines and standards in an effort to address some of the areas of local concern. Use of this study will provide a realistic overview of the current conditions of a portion of the Santa Paula Redevelopment Project Area and establish a foundation from which existing physical, social and economic conditions can be improved over the next decade.

Upon review of the downtown area, significant joint funding redevelopment upgrading potentials are clearly evident. Cooperation between private and public sectors is essential in realizing these market opportunities. This joint effort will ensure that the Downtown Improvement Plan will reflect the objectives of the downtown community, since it is essential that the program determine primary issues, opportunities, constraints, policies, and development assumptions. Such an objective setting will open the door to a development strategy, which will be accepted for local adoption.

Responsibility for carrying out downtown improvements will rest in the hands of numerous individuals and organizations, including private businesses and public agencies. Furthermore, since the financial resources with which to execute these projects will always be limited, prioritizing of projects in a systematic manner is of the utmost importance.

Improvements which have been recommended are a direct result of the joint efforts of the Downtown Improvement Group and the Technical Design Team. Due to the multitude of needed projects, it is essential that a carefully organized program for the execution of the downtown improvements be stated. The timetable or phased implementation approach contained herein is based on the availability of funding as well as personnel and the successful completion of the related projects.

Many of the projects are short-range (between 1-5 years) while the estimated completion date for others is mid-range (between 5-10 years) and long-range (between 10-15 years). The initial projects undertaken should be those with a highly visible impact. Hopefully, the successful implementation of these efforts will induce future downtown improvements and serve to stimulate further efforts

of the private and public sectors of the community. These time tables should be viewed as estimates. The early completion of all of the projects would be welcome.

Financing public redevelopment improvements is typically an obstacle to successful completion of a project due to problems acquiring long-term financing from conventional lending institutions. Loans for design improvements relating to rehabilitation are often difficult to obtain as those borrowers may have a less secure financial status than developers doing new construction. Therefore, the City/Agency must evaluate various financial sources, which could assist in the funding, as well as commercial revitalization programs, which leverage private monies with public funds. These sources of funds include the following:

- ▣ Tax Increment Financing
- ▣ Commercial Rehabilitation Programs and Grants
- ▣ Special Assessment Districts
- ▣ Small Business Administration (SBA) Loans
- ▣ Ongoing City Capital Improvement Funds
- ▣ Fund Raisers Sponsored by Service Groups and Downtown Merchants
- ▣ Loans and Grants from Non-Profit Organizations
- ▣ Developer Agreements (DDA)
- ▣ Owner Participation Agreements (OPA)
- ▣ Facade Agreements

Other formal ways of upgrading the aesthetics of downtown Santa Paula may include the provision of special signage guidelines for the downtown area. This could be accomplished by revising the current City Zoning Ordinance and could be implemented through a design review process.

Given the potential number and quality of historic and significant buildings within the community, it is also suggested that the City seriously consider updating formal measures which would encourage the preservation of these buildings for future generations. This may be accomplished by a combination of actions including:

- ▣ Updating the Architectural/Historic Survey, which may be used as base information for future grant applications;
- ▣ Prepare a National Register Historic District Nomination for downtown Santa Paula;
- ▣ Update of the current Historic District; and

- Loan and grant programs specifically designated for the purpose of historic preservation. A National Register District would economically benefit business owners by making them eligible for grants and tax writeoffs.

Due to the restrictions of some financing programs, additional financial burdens may be imposed on private property owners, as a condition of financing approval.

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PLANNING RECOMMENDATIONS ACCESS & CIRCULATION PROJECT	PUBLIC SECTOR PARTICIPANTS	PRIVATE SECTOR PARTICIPANTS	SHORT TERM 1-5 YEARS	MID TERM 5-10 YEARS	LONG TERM 10-15 YEARS
Parking lot rehabilitation and landscaping	●	●	★		
Angle parking	■	□	★		
Alley rehabilitation	●	●	★		
Pedestrian accessways	●	●	★		★
Access to railroad and airport	●	●			★
Tram service	●	●			★

LEGEND: ● JOINT EFFORT ■ PRIMARY INVOLVEMENT □ SECONDARY INVOLVEMENT ★ TIME FRAME

PLANNING RECOMMENDATIONS BUILDING RESTORATION PROGRAM	PUBLIC SECTOR PARTICIPANTS	PRIVATE SECTOR PARTICIPANTS	SHORT TERM 1-5 YEARS	MID TERM 5-10 YEARS	LONG TERM 10-15 YEARS
Facade restoration	□	■	★	★	
Historic preservation	□	■	★	★	
Seismic rehabilitation	●	●	★	★	
Seed money for engineering and architecture	■	□	★	★	
Long-term financing/refinancing	□	■	★	★	★
Architectural guidelines	●	●	★		

LEGEND: ● JOINT EFFORT ■ PRIMARY INVOLVEMENT □ SECONDARY INVOLVEMENT ★ TIME FRAME

PLANNING RECOMMENDATIONS COMMERCIAL REDEVELOPMENT PROGRAM	PUBLIC SECTOR PARTICIPANTS	PRIVATE SECTOR PARTICIPANTS	SHORT TERM 1-5 YEARS	MID TERM 5-10 YEARS	LONG TERM 10-15 YEARS
Museums and attractions	●	●	★	★	
Infill vacant sites	□	■	★	★	
South Tenth Street redevelopment	●	●	★	★	
Railroad Avenue redevelopment	□	■	★	★	
Project assistance	●	●	★	★	

LEGEND: ● JOINT EFFORT ■ PRIMARY INVOLVEMENT □ SECONDARY INVOLVEMENT ★ TIME FRAME

PLANNING RECOMMENDATIONS GREEN STREET PLAZA & PARK DEVELOPMENT PROJECT	PUBLIC SECTOR PARTICIPANTS	PRIVATE SECTOR PARTICIPANTS	SHORT TERM 1-5 YEARS	MID TERM 5-10 YEARS	LONG TERM 10-15 YEARS
Green Street accessway	■	□	★		
Green Street plaza	■	□	★		
New park at Eleventh and Main Streets	■		★		
Expansion of Veterans Memorial Park Civic Center	■			★	
Civic Center grounds improvements	■			★	

LEGEND: ● **JOINT EFFORT** ■ **PRIMARY INVOLVEMENT** □ **SECONDARY INVOLVEMENT** ★ **TIME FRAME**

PLANNING RECOMMENDATIONS RAILROAD RIGHT-OF-WAY PROJECT	PUBLIC SECTOR PARTICIPANTS	PRIVATE SECTOR PARTICIPANTS	SHORT TERM 1-5 YEARS	MID TERM 5-10 YEARS	LONG TERM 10-15 YEARS
Public ownership	■		★		
Park and trail	■		★	★	
Tourist train station and siding	●	●	★		
Street closures	■		★		
Commercial development	□	■	★	★	
Space for future Metrolink station	■			★	

LEGEND: ● JOINT EFFORT ■ PRIMARY INVOLVEMENT □ SECONDARY INVOLVEMENT ★ TIME FRAME

PLANNING RECOMMENDATIONS STREETSCAPE PROJECT	PUBLIC SECTOR PARTICIPANTS	PRIVATE SECTOR PARTICIPANTS	SHORT TERM 1-5 YEARS	MID TERM 5-10 YEARS	LONG TERM 10-15 YEARS
Historic street lights with banners	■	<input type="checkbox"/>	★		
Street furniture	■	<input type="checkbox"/>	★		
Signage	■	<input type="checkbox"/>	★		
Tree control or replacement	■		★	★	
Spot landscaping at key locations	■	<input type="checkbox"/>	★	★	
Tenth Street widening	■		★		★

LEGEND: ● JOINT EFFORT ■ PRIMARY INVOLVEMENT □ SECONDARY INVOLVEMENT ★ TIME FRAME